



## INCIDENTAL HARASSMENT AUTHORIZATION

The U.S. Navy (Navy), Naval Station Mayport, Florida, is hereby authorized under section 101(a)(5)(D) of the Marine Mammal Protection Act (MMPA; 16 U.S.C. 1371(a)(5)(D)), to harass marine mammals incidental to the Wharf C-2 Recapitalization Project at Mayport, FL.

1. This Incidental Harassment Authorization (IHA) is valid for one year from the date of issuance.
2. This IHA is valid only for pile driving activities associated with the Wharf C-2 Recapitalization Project at Naval Station Mayport, Florida.
3. General Conditions
  - (a) A copy of this IHA must be in the possession of the Navy, its designees, and work crew personnel operating under the authority of this IHA.
  - (b) The species authorized for taking is the bottlenose dolphin (*Tursiops truncatus truncatus*).
  - (c) The taking, by Level B harassment only, is limited to the species listed in condition 3(b). See Table 1 (attached) for numbers of take authorized.
  - (d) The taking by injury (Level A harassment), serious injury, or death of any of the species listed in item 3(b) of the Authorization or any taking of any other species of marine mammal is prohibited and may result in the modification, suspension, or revocation of this IHA.
  - (e) The Navy shall conduct briefings between construction supervisors and crews, marine mammal monitoring team, and Navy staff prior to the start of all pile driving activity, and when new personnel join the work, in order to explain responsibilities, communication procedures, marine mammal monitoring protocol, and operational procedures.

4. Mitigation Measures

The holder of this Authorization is required to implement the following mitigation measures:

- (a) For all pile driving, the Navy shall implement a minimum shutdown zone of 15 m radius around the pile. If a marine mammal comes within or approaches the shutdown zone, such operations shall cease. For impact driving of steel piles, the minimum shutdown zone shall be of 40 m radius.
- (b) The Navy shall establish monitoring locations as described below. Please also refer to the Marine Mammal Monitoring Plan (Monitoring Plan; attached).



- (i) For all pile driving activities, a minimum of two observers shall be deployed, with one positioned to achieve optimal monitoring of the shutdown zone and the second positioned to achieve optimal monitoring of surrounding waters of the turning basin, the entrance to that basin, and portions of the Atlantic Ocean. The second observer shall be deployed to an elevated position opposite Wharf C-2 and with clear sight lines to the wharf and out the entrance channel.
  - (ii) For three of the days on which vibratory pile driving activities are conducted, a third observer shall be positioned for visual observation of waters outside the turning basin, including the entrance to that basin and portions of the Atlantic Ocean that are within the predicted area of ensonification and are visible with binoculars and the naked eye.
  - (iii) These observers shall record all observations of marine mammals, regardless of distance from the pile being driven, as well as behavior and potential behavioral reactions of the animals. Observations within the turning basin shall be distinguished from those in the entrance channel and nearshore waters of the Atlantic Ocean.
  - (iv) All observers shall be equipped for communication of marine mammal observations amongst themselves and to other relevant personnel (e.g., those necessary to effect activity delay or shutdown).
- (c) Monitoring shall take place from 15 minutes prior to initiation of pile driving activity through 30 minutes post-completion of pile driving activity. Pre-activity monitoring shall be conducted for 15 minutes to ensure that the shutdown zone is clear of marine mammals, and pile driving may commence when observers have declared the shutdown zone clear of marine mammals. In the event of a delay or shutdown of activity resulting from marine mammals in the shutdown zone, animals shall be allowed to remain in the shutdown zone (i.e., must leave of their own volition) and their behavior shall be monitored and documented. Monitoring shall occur throughout the time required to drive a pile. The shutdown zone must be determined to be clear during periods of good visibility (i.e., the entire shutdown zone and surrounding waters must be visible to the naked eye).
- (d) If a marine mammal approaches or enters the shutdown zone, all pile driving activities at that location shall be halted. If pile driving is halted or delayed due to the presence of a marine mammal, the activity may not commence or resume until either the animal has voluntarily left and been visually confirmed beyond the shutdown zone or 15 minutes have passed without re-detection of the animal.
- (e) Monitoring shall be conducted by qualified observers, as described in the Monitoring Plan. Trained observers shall be placed from the best vantage point(s) practicable to monitor for marine mammals and implement shutdown or delay procedures when applicable through communication with the equipment operator. Observer training must be provided prior to project start and in accordance with the monitoring plan, and shall include instruction on species identification

(sufficient to distinguish the species listed in 3(b)), description and categorization of observed behaviors and interpretation of behaviors that may be construed as being reactions to the specified activity, proper completion of data forms, and other basic components of biological monitoring, including tracking of observed animals or groups of animals such that repeat sound exposures may be attributed to individuals (to the extent possible):

- (f) The Navy shall use soft start techniques recommended by NMFS for impact pile driving. The soft start requires contractors to provide an initial set of strikes from the impact hammer at reduced energy, followed by a 30-second waiting period, then two subsequent reduced energy strike sets. Soft start shall be implemented at the start of each day's impact pile driving and at any time following cessation of impact pile driving for a period of 30 minutes or longer.
- (g) Pile driving shall only be conducted during daylight hours.

#### 5. Monitoring

The holder of this Authorization is required to conduct acoustic and marine mammal monitoring during pile driving activity. Monitoring and reporting shall be conducted in accordance with the Monitoring Plan.

- (a) The Navy shall collect sighting data and behavioral responses to pile driving for marine mammal species observed in the region of activity during the period of activity. All observers shall be trained in marine mammal identification and behaviors, and shall have no other construction related tasks while conducting monitoring.
- (b) For all marine mammal monitoring, the information shall be recorded as described in the Monitoring Plan.

#### 6. Reporting

The holder of this Authorization is required to:

- (a) Submit a draft report on all monitoring conducted under the IHA within 90 days of the completion of marine mammal monitoring. A final report shall be prepared and submitted within 30 days following resolution of comments on the draft report from NMFS. This report must contain the informational elements described in the Monitoring Plan, at minimum (see attached), and shall also include:
  - (i) Detailed information about any implementation of shutdowns, including the distance of animals to the pile and description of specific actions that ensued and resulting behavior of the animal, if any.
  - (ii) Description of attempts to distinguish between the number of individual animals taken and the number of incidences of take, such as ability to track groups within turning basin.
  - (iii) A refined take estimate based on the number of marine mammals observed during the course of construction activities.

- (b) Submit a report summarizing the acoustic monitoring data within 75 days of the completion of monitoring. A final report shall be prepared and submitted within 30 days following resolution of comments on the draft report from NMFS. This report must contain the informational elements described in the Monitoring Plan, at minimum (see attached).
- (c) Reporting injured or dead marine mammals:
- (i) In the unanticipated event that the specified activity clearly causes the take of a marine mammal in a manner prohibited by this IHA, such as an injury (Level A harassment), serious injury, or mortality, Navy shall immediately cease the specified activities and report the incident to the Chief of the Permits and Conservation Division (301-427-8425), Office of Protected Resources, NMFS, and the Southeast Regional Stranding Coordinator (305-361-4586), NMFS. The report must include the following information:
1. Time and date of the incident;
  2. Description of the incident;
  3. Environmental conditions (e.g., wind speed and direction, Beaufort sea state, cloud cover, and visibility);
  4. Description of all marine mammal observations in the 24 hours preceding the incident;
  5. Species identification or description of the animal(s) involved;
  6. Fate of the animal(s); and
  7. Photographs or video footage of the animal(s).

Activities shall not resume until NMFS is able to review the circumstances of the prohibited take. NMFS will work with Navy to determine what measures are necessary to minimize the likelihood of further prohibited take and ensure MMPA compliance. Navy may not resume their activities until notified by NMFS.

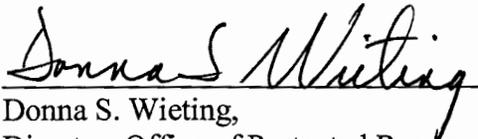
- (ii) In the event that Navy discovers an injured or dead marine mammal, and the lead observer determines that the cause of the injury or death is unknown and the death is relatively recent (e.g., in less than a moderate state of decomposition), Navy shall immediately report the incident to the Chief of the Permits and Conservation Division, Office of Protected Resources, NMFS, and the Southeast Regional Stranding Coordinator, NMFS.

The report must include the same information identified in 6(b)(i) of this IHA. Activities may continue while NMFS reviews the circumstances of

the incident. NMFS will work with Navy to determine whether additional mitigation measures or modifications to the activities are appropriate.

- (iii) In the event that Navy discovers an injured or dead marine mammal, and the lead observer determines that the injury or death is not associated with or related to the activities authorized in the IHA (e.g., previously wounded animal, carcass with moderate to advanced decomposition, or scavenger damage), Navy shall report the incident to the Chief of the Permits and Conservation Division, Office of Protected Resources, NMFS, and the Southeast Regional Stranding Coordinator, NMFS, within 24 hours of the discovery. Navy shall provide photographs or video footage or other documentation of the stranded animal sighting to NMFS.

- 7. This Authorization may be modified, suspended or withdrawn if the holder fails to abide by the conditions prescribed herein, or if NMFS determines the authorized taking is having more than a negligible impact on the species or stock of affected marine mammals.



Donna S. Wieting,  
Director, Office of Protected Resources,  
National Marine Fisheries Service.

SEP 08 2015

Date

**Table 1. Authorized take numbers, by species**

<b>Species</b>	<b>Authorized Take</b>
Bottlenose dolphin ( <i>Tursiops truncatus</i> )	304

---

**DRAFT MARINE MAMMAL MONITORING PLAN  
FOR THE WHARF C-2 RECAPITALIZATION PROJECT  
AT NAVSTA MAYPORT, FLORIDA  
NAVY REGION SOUTHEAST (YEAR 2)**

---



**Submitted to:**

Office of Protected Resources,  
National Marine Fisheries Service,  
National Oceanographic and Atmospheric Administration

**Prepared by:**

Naval Facilities Engineering Command Southeast  
and  
Naval Facilities Engineering Command Atlantic

July 2015

THIS PAGE INTENTIONALLY LEFT BLANK

## **TABLE OF CONTENTS**

<b>1.0 Introduction.....</b>	<b>1</b>
1.1 Purpose of the Monitoring Plan.....	1
1.2 Scope and Timing.....	1
1.3 Management .....	1
<b>2.0 Wharf C-2 Recapitalization Project.....</b>	<b>3</b>
2.1 Project Area.....	3
2.2 Activities to be Monitored.....	3
2.3 Monitoring and Shutdown Zones .....	7
<b>3.0 Marine Mammal Monitoring.....</b>	<b>9</b>
3.1 Observers and Procedures .....	9
3.2 Methods .....	9
3.3 Data Collection.....	10
3.4 Equipment .....	11
3.5 Observer Monitoring Locations .....	11
3.6 Interagency Notification.....	11
<b>4.0 Reporting.....</b>	<b>12</b>
<b>5.0 References.....</b>	<b>13</b>

**LIST OF FIGURES**

Figure 1-1. Regional Location – Naval Station Mayport, Mayport, Florida .....2

Figure 2-1. Injury and Behavioral Zones of Influence for Marine Mammals – Vibratory Driving of Steel King and Sheet Piles .....4

Figure 2-2. Injury and Behavioral Zones of Influence for Marine Mammals – Impact Driving of Steel King and Sheet Piles (Contingency Only).....5

Figure 2-3. Behavioral Zone of Influence for Marine Mammals – Vibratory Driving of Polymeric Piles .....6

Figure 2-4. Monitoring / Shutdown Zone.....8

**LIST OF TABLES**

Table 2-1. Monitoring and Shutdown Zones.....7

## **ACRONYMS AND ABBREVIATIONS**

C-2	Charlie Two (Wharf)
dB	decibel
EA	Environmental Assessment
ft.	foot / feet
IHA	Incidental Harassment Authorization
μPa	microPascal
m	meter
MMPA	Marine Mammal Protection Act
NAVSTA	Naval Station
NEPA	National Environmental Policy Act
NMFS	National Marine Fisheries Service
POC	point of contact
Project	Wharf C-2 Recapitalization Project
PTS	Permanent Threshold Shift
TTS	Temporary Threshold Shift
USFWS	U.S. Fish and Wildlife Service
ZOI	Zone of Influence

## **1.0 INTRODUCTION**

### **1.1 Purpose of the Monitoring Plan**

The purpose of this Monitoring Plan is to provide protocols for marine mammal monitoring during the proposed recapitalization of Wharf Charlie Two (C-2) at Naval Station (NAVSTA) Mayport, Florida (Figure 1-1). Recapitalization includes demolishing and replacing the existing concrete pile cap, wharf deck, and utilities and installation of a new steel king pile/sheet pile bulkhead around the existing wharf. The project also includes installation of polymeric fender piles along the new wharf bulkhead via vibratory driving. This plan was developed to support the National Marine Fisheries (NMFS) Incidental Harassment Authorization (IHA) Application (U.S. Department of the Navy 2015).

Marine mammal monitoring will be conducted before, during, and after pile driving activities within the zones detailed in Section 2.3, and will represent an important minimization measure to reduce the likelihood of potential injury to marine mammals.

### **1.2 Scope and Timing**

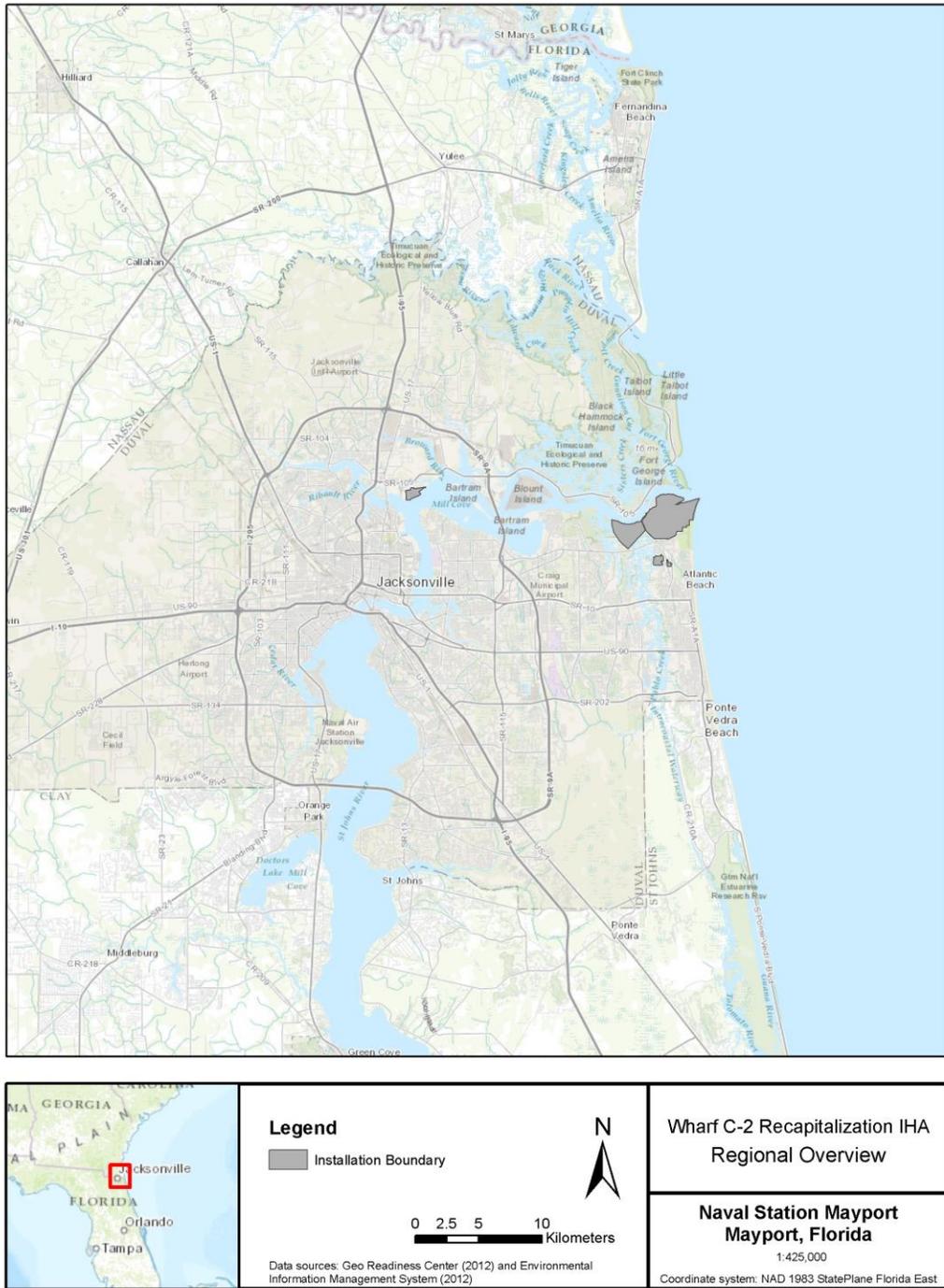
The scope of this Monitoring Plan includes pile driving activities that are necessary for the Wharf C-2 recapitalization project (Project). Sea turtles and smalltooth sawfish (as practicable) will be included in monitoring efforts. However, for the purposes of this submittal to NMFS in support of compliance with the Marine Mammal Protection Act (MMPA), the scope of monitoring in this document is limited to marine mammals. Marine mammal monitoring would be integrated with other marine environmental monitoring if it is required as a result of the Navy's National Environmental Policy Act (NEPA) project review or as a condition of approval by other regulatory agencies.

This Monitoring Plan will be implemented when pile driving is taking place during the period of the requested IHA (1 September 2015 through 31 August 2016) for the Project.

### **1.3 Management**

The Monitoring Plan will be managed by Naval Facilities Engineering Command (NAVFAC) Southeast. Marine mammal monitoring will be carried out by private contractors supported by local technical staff from NAVFAC Southeast and NAVSTA Mayport. NAVFAC Southeast will also be responsible for preparation of the Monitoring Report for the IHA.

**Figure 1-1. Regional Location – Naval Station Mayport, Mayport, Florida**



## **2.0 WHARF C-2 RECAPITALIZATION PROJECT**

Refer to the Environmental Assessment (EA) (U.S. Department of the Navy 2013b), previous and current IHA Applications (U.S. Department of the Navy 2013a, 2015), and previously issued IHA (National Marine Fisheries Service 2013) for a full description of the Project.

### **2.1 Project Area**

The project area is along the Atlantic coast of northern Florida, and includes the NAVSTA Mayport turning basin out to the limit of the most distant of the acoustic thresholds for all protected species being addressed for the Project (Figure 2-1). The lesser acoustic threshold distances are displayed in Figures 2-2 and 2-3. Acoustic thresholds used in this monitoring report are based on criteria developed by NMFS<sup>1</sup> (70 FR 1871; 74 FR 41684).

### **2.2 Activities to be Monitored**

Activities which would be subject to marine mammal monitoring include the following:

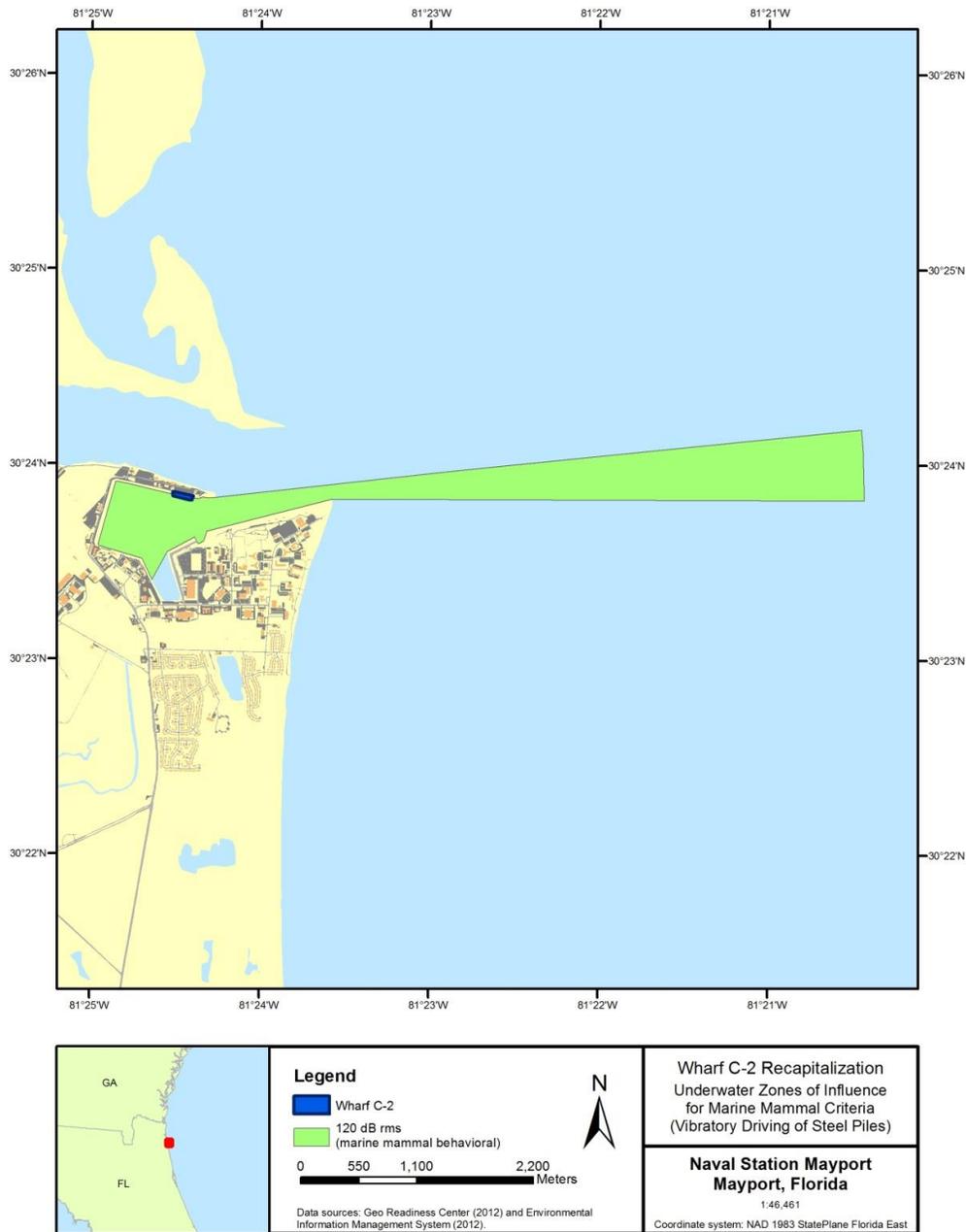
- Vibratory pile driving of steel king and sheet piles necessary to construct a new steel sheet pile wall outside the existing bulkhead. Approximately 36 steel sheet pile pairs and 38 steel king piles will be installed with a vibratory driver.
- Vibratory installation of 50 polymeric (plastic) fender piles.
- Contingency-only impact installation polymeric fender piles. Impact driving will only be used if vibratory driving is inadequate or an obstruction that prevents vibratory installation of is encountered.

Marine mammal monitoring will be performed to ensure that in-water activities are stopped if animals occur within the zone of influence (ZOI) for potential injury or a standard 50 feet (ft.) buffer from pile driving activities (Figure 2-4). Monitoring methods are described in Section 3 of this document.

---

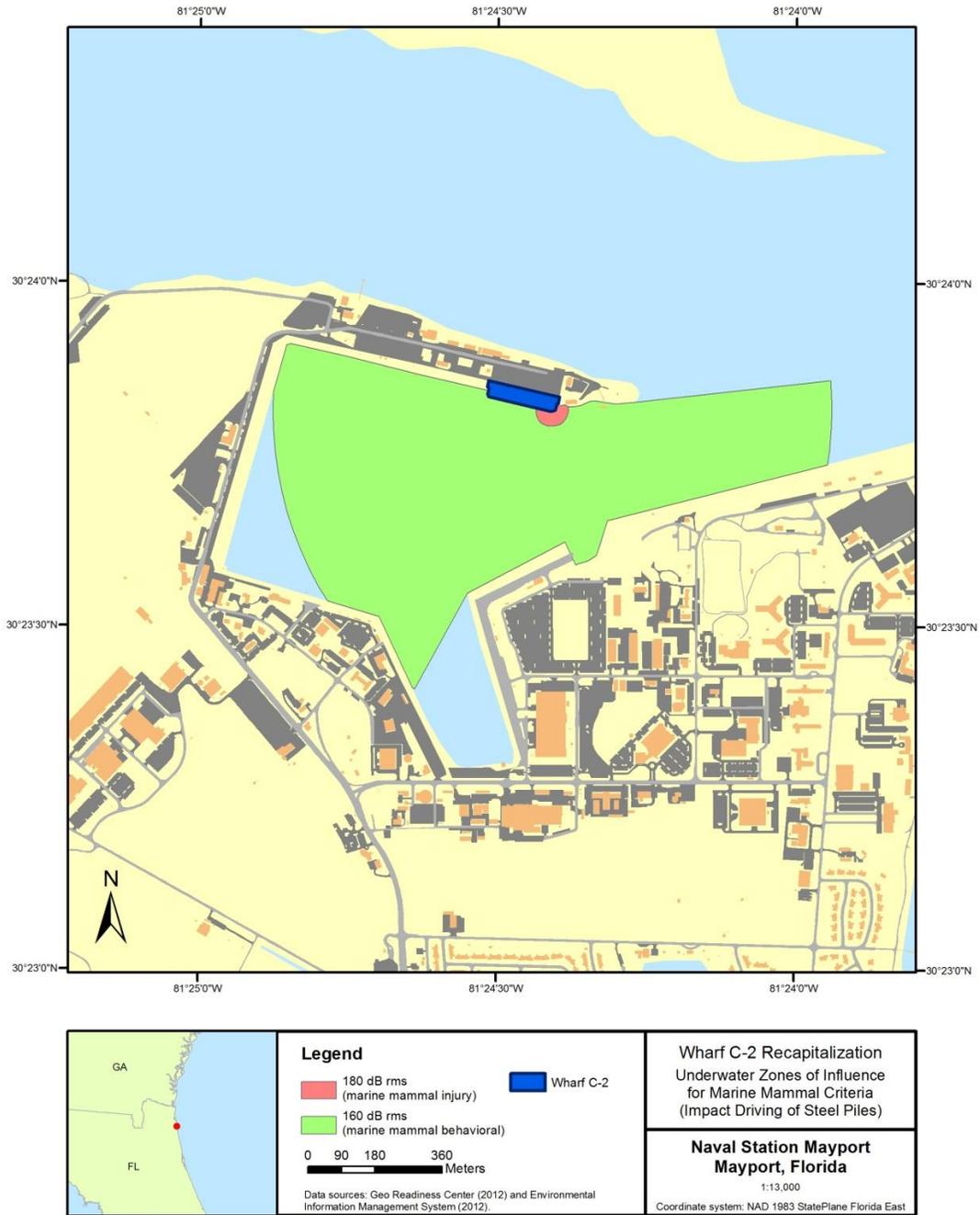
<sup>1</sup> New acoustic criteria covering permanent and temporary threshold shifts (PTS and TTS, respectively) were proposed by NMFS in December 2013. At the time of submittal, these criteria have not been finalized and no implementation guidance has been issued. They are therefore not addressed in this mitigation plan.

**Figure 2-1. Injury and Behavioral Zones of Influence for Marine Mammals<sup>2</sup> – Vibratory Driving of Steel King and Sheet Piles**



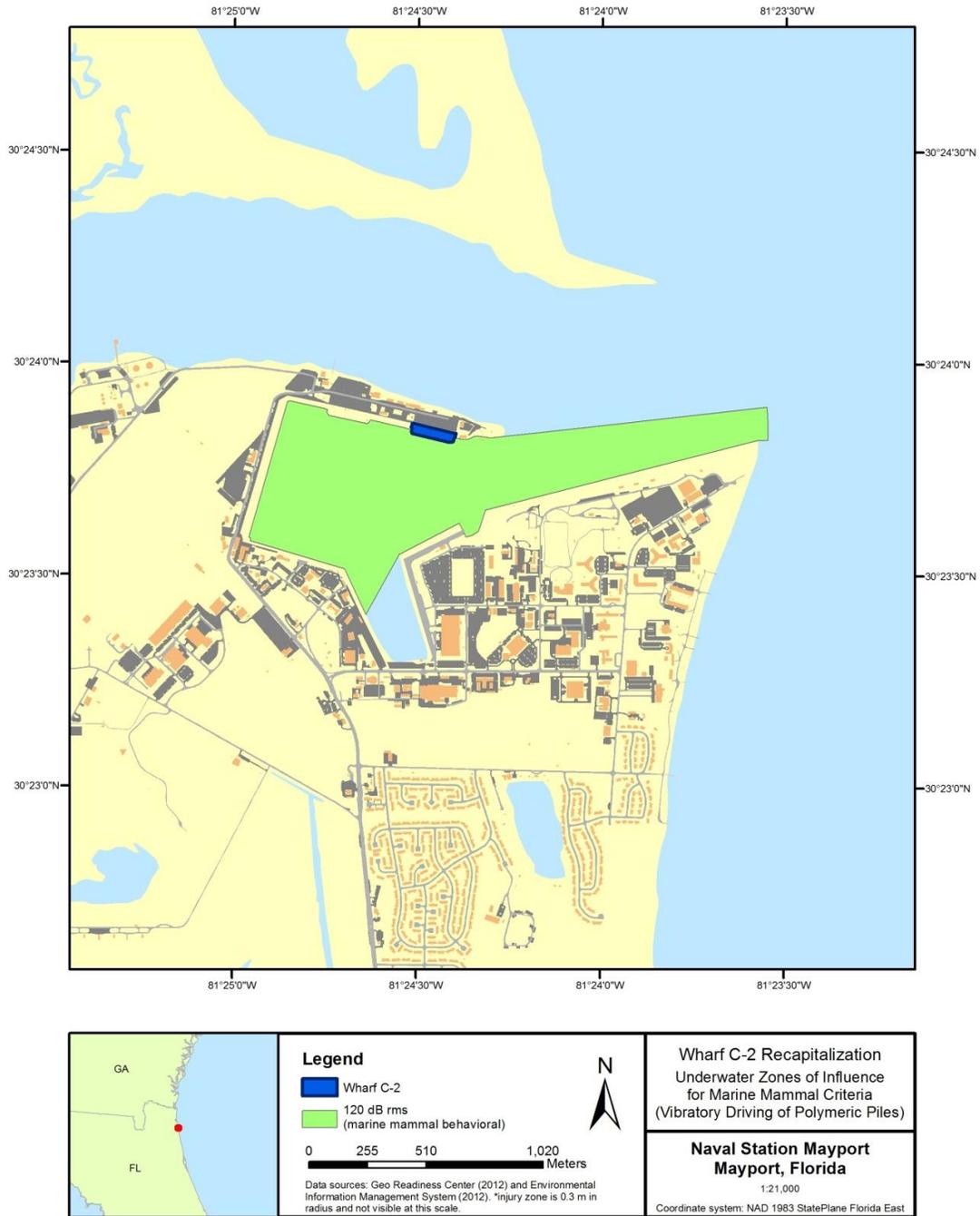
<sup>2</sup> Official criteria have not been established for West Indian manatees. The Navy’s IHA application, Appendix C – Standards Manatee Conditions for In-Water Work, cover standards of practice promulgated by The U.S. Fish and Wildlife Service (USFWS) for manatees.

**Figure 2-2. Injury and Behavioral Zones of Influence for Marine Mammals<sup>3</sup> – Impact Driving of Steel King and Sheet Piles (Contingency Only)**



<sup>3</sup> Official criteria have not been established for West Indian manatees; marine mammal injury zone of influence illustrated represents a notional template location

**Figure 2-3. Behavioral Zone of Influence for Marine Mammals<sup>4</sup> – Vibratory Driving of Polymeric Piles**



<sup>4</sup> Official criteria have not been established for West Indian manatees; marine mammal injury zone of influence illustrated represents a notional template location

### Pile Installation

The acoustic analysis for vibratory pile driving used the assumption a maximum of three templates (each consisting of five king piles and four sheet pile pairs) would be driven each day. Each pile is anticipated to require no more than 60 seconds to drive by vibratory methods. However, during Year 1 of the Project, the contractor reported that sheet pile installation has taken between 10 and 20 seconds per pile and king pile installation has taken between 10 and 30 minutes per pile. Impact pile driving would only be used as a contingency in cases when vibratory driving is insufficient (a similar project that has been completed at adjacent Wharf Charlie One required impact pile driving on only seven piles). Installation of polymeric piles will also be conducted via vibratory pile driving. Approximately five piles would be driven each day. Similarly, impact driving will only be used if obstructions are encountered, making vibratory driving inadequate.

### 2.3 Monitoring and Shutdown Zones

Table 2-1 lists the monitoring and shutdown zones, and measures associated with the occurrence of a marine mammal in each zone. For all in-water construction and demolition activities, a minimum protective shutdown zone of 15 m (50 ft.) is proposed. Sound-generating activities with larger shutdown zones follow, based on the maximum modeled distance to the Level A (injury) threshold:

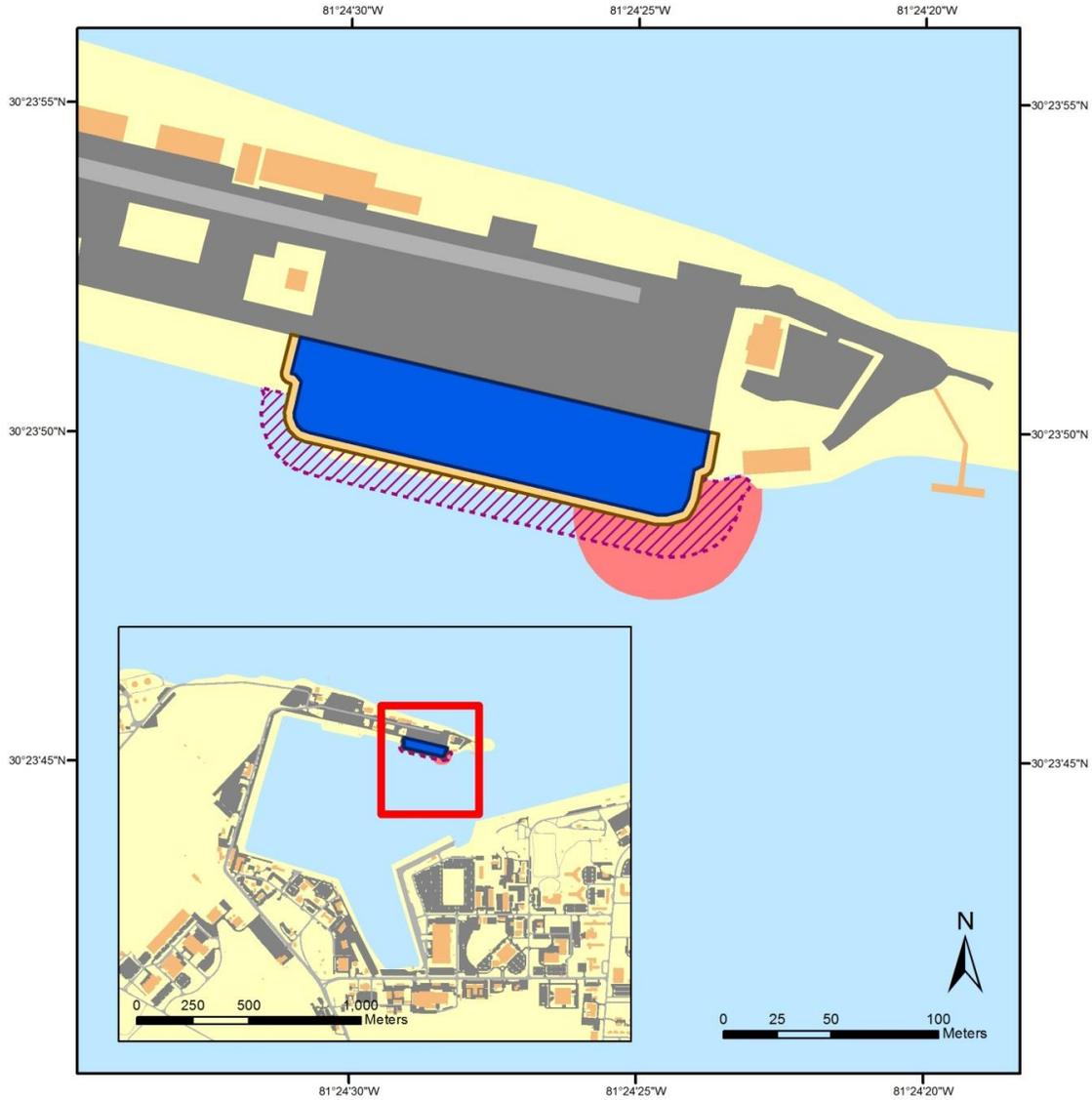
- During vibratory pile driving, the shutdown distance will initially be 15 m.
- If impact driving of steel piles is needed, the shutdown distance for cetaceans will be 40 m during the brief duration of such activities.

**Table 2-1. Monitoring and Shutdown Zones**

Type of Activity	Distance from Pile Being Driven and Active In-water Equipment (any direction in water)	Measure
All in-water work <sup>1</sup>	50 ft. (15 m)	Shut down all in-water work if a marine mammal, sea turtle, or smalltooth sawfish (surface) is observed in the zone
Impact driving of steel piles (contingency only)	130 ft. (40 m)	Shut down pile driving if a marine mammal is observed in the zone

<sup>1</sup> In-water work is defined as any activity where personnel or equipment are working in the water column. Vessel movement does not constitute in-water work.

**Figure 2-4. Monitoring / Shutdown Zone**



	<p><b>Legend</b></p> <ul style="list-style-type: none"> <li><span style="display: inline-block; width: 15px; height: 15px; background-color: blue; border: 1px solid black; margin-right: 5px;"></span> Wharf C-2</li> <li><span style="display: inline-block; width: 15px; height: 15px; background-color: yellow; border: 1px solid black; margin-right: 5px;"></span> Wharf C-2 New Profile</li> <li><span style="display: inline-block; width: 15px; height: 15px; border: 1px dashed purple; margin-right: 5px;"></span> Protected Species Shutdown Zone (any vibratory pile driving)</li> <li><span style="display: inline-block; width: 15px; height: 15px; background-color: red; border: 1px solid red; margin-right: 5px;"></span> Notional Marinal Mammal Injury Zone (steel impact pile driving)</li> </ul> <p><small>Data sources: Geo Readiness Center (2012) and Environmental Information Management System (2012)</small></p>	<p>Wharf C-2 Recapitalization</p> <p>Protected Species Shutdown Zone</p>
		<p><b>Naval Station Mayport</b> <b>Mayport, Florida</b></p> <p>1:2,500</p> <p><small>Coordinate system: NAD 1983 StatePlane Florida East</small></p>

### **3.0 MARINE MAMMAL MONITORING**

#### **3.1 Observers and Procedures**

The Navy shall conduct a pre-construction briefing with the contractor. During the briefing, all contractor personnel working in the Project area will watch the Navy's Marine Species Awareness Training presentation.

Marine mammal observers ("observers") designated by the contractor will be placed at the best vantage point(s) practicable to monitor for marine mammals and implement shutdown/delay procedures when applicable by calling for the shutdown to the hammer operator. The observers will have no other construction related tasks while conducting monitoring.

The contractor will adhere to all requirements of the following:

- U.S. Fish and Wildlife Services (USFWS) 2005 Standard Manatee Conditions for In-Water Work (Attachment 1)
- National Marine Fisheries Service 2006 Sea Turtle and Smalltooth Sawfish Construction Conditions (Attachment 2)
- National Marine Fisheries Services 2012 Southeast Region Marine Mammal and Sea Turtle Viewing Guidelines (Attachment 3)
- Requirements of IHA upon issuance by NMFS.

#### **3.2 Methods**

The observer(s) will monitor the shutdown zone before, during, and after pile driving and removal.

The observer will be placed at the best vantage point practicable (e.g. from a small boat, construction barges, on shore, or any other suitable location) to monitor for marine mammals and implement shutdown/delay procedures when applicable by calling for the shutdown to the equipment operator(s). Elevated positions are preferable; it shall be the contractor's responsibility to ensure that appropriate safety measures are implemented to protect observers on elevated observation points. If a boat is used for monitoring, the boat will maintain minimum distances from species (should they occur) as described in National Marine Fisheries Services' 2012 Southeast Region Marine Mammal and Sea Turtle Viewing Guidelines (Attachment 3).

- During all observation periods, observers would use binoculars and the naked eye to search continuously for marine mammals;
- If the shutdown zone is obscured by fog or poor lighting conditions, pile driving will not be initiated until the entire shutdown zone is visible.
- The shutdown zone will be monitored for the presence of marine mammals before, during, and after any pile driving or removal activity.

#### Pre-Activity Monitoring:

The shutdown zone will be monitored for 15 minutes prior to in-water construction/demolition activities. If a marine mammal is present within or approaching the shutdown zone, the activity would be delayed until the animal(s) leave the shutdown zone. Activity would resume only after the observer has determined, through re-sighting or by waiting approximately 15 minutes that the animal(s) has moved outside the shutdown zone. The observer will notify the monitoring coordinator/construction foreman / POC when construction activities can commence.

#### During Activity Monitoring:

The shutdown zone shall include all areas where the underwater sound pressure levels are anticipated to equal or exceed the Level A (injury) criteria for marine mammals (180 dB re 1  $\mu$ Pa isopleth for cetaceans). The shutdown zone will always be a minimum of 15 meters (m) (50 ft.) to prevent injury from physical interaction of marine mammals with construction equipment (Figure 2-2).

If a marine mammal, sea turtle, or smalltooth sawfish enters a shutdown zone during any in-water work, activity will be halted and delayed until either the animal has voluntarily left and been visually confirmed beyond the shutdown zone or 30 minutes have passed without re-detection of the animal.

#### Post-Activity Monitoring:

Monitoring of the shutdown zone will continue for 30 minutes following the completion of the activity.

### **3.3 Data Collection**

The following information will be collected on sighting forms used by observers:

- Date and time that pile driving or removal begins or ends
- Construction activities occurring during each observation period
- Weather parameters identified in the acoustic monitoring (e.g., wind, temperature, percent cloud cover, and visibility)
- Tide state and water currents

If a marine mammal, sea turtle, or smalltooth sawfish enters the shutdown zone, the following information will be recorded once shutdown procedures have been implemented:

- Species, numbers, and if possible sex and age class of marine mammals
- Behavior patterns observed, including bearing and direction of travel
- Location of the observer and distance from the animal(s) to the observer

If possible, photographs of the animal(s) will be taken and forwarded to the Naval Facilities Engineering Command Southeast Environmental point of contact.

Data collection forms shall be furnished to the Environmental point of contact within a mutually agreeable timeframe.

### **3.4 Equipment**

The observer(s) shall be equipped with the following:

- binoculars (7 x 50 power or greater) to ensure sufficient visual acuity while investigating sightings
- portable radios or cellular phone(s) to rapidly communicate with the appropriate construction personnel to initiate shutdown of pile driving activity if required
- a digital camera for photographing any marine species sighted
- data collection forms
- Compass/GPS

### **3.5 Observer Monitoring Locations**

In order to effectively monitor the shutdown zones, marine mammal observers will be positioned at the best practicable vantage point(s), taking into consideration the behavior of marine mammal species likely to enter the area, security, safety, and space limitations at the waterfront, in order to properly monitor these zones. Observers may be stationed in small vessels or on the wharf at a location that will provide adequate visual coverage for the marine mammal shutdown zone.

### **3.6 Interagency Notification**

If the Navy encounters an injured, sick, or dead marine mammal, NMFS will be notified immediately. Such sightings will be called into the NMFS Stranding Coordinator for the Southeast:

Erin Fougères, Ph.D.  
Marine Mammal Stranding Program Administrator  
NOAA Fisheries  
Southeast Regional Office  
263 13th Avenue South  
St. Petersburg, FL 33701  
e-mail: [erin.fougeres@noaa.gov](mailto:erin.fougeres@noaa.gov)  
office: 727-824-5323  
fax: 727-824-5309

The Navy will provide NMFS with the species or description of the animal(s), the condition of the animal (including carcass condition if the animal is dead), location, the date and time of first discovery, observed behaviors (if alive), and photo or video (if available).

Care should be taken in handling dead specimens to preserve biological materials in the best possible state for later analysis of cause of death, if that occurs. In preservation of biological materials from a dead animal, the finder (i.e. marine mammal observer) has the responsibility to ensure that evidence associated with the specimen is not unnecessarily disturbed.

#### **4.0 REPORTING**

A draft report of any incidents of marine mammals entering the shutdown zone will be forwarded to NMFS / USFWS no later than 31 November 2016. A final report would be prepared and submitted to NMFS within 30 days following receipt of comments on the draft report from NMFS.

## **5.0 REFERENCES**

- Hannigan, P. (2011). Pile Driving Equipment. 2011 PDCA Professor Pile Institute. Produced by GRL Engineers, Inc. Retrieved from <http://www.piledrivers.org/pdpi-pat-hannigan.htm>. Accessed on 04 November 2012
- National Marine Fisheries Service. (2013). Incidental Harassment Authorization for Wharf C-2 Recapitalization Project at Mayport, FL. Issued 25 November 2013.
- U.S. Department of the Navy. (2013a). Request for an Incidental Harassment Authorization Under the Marine Mammal Protection Act for the Wharf C-2 Recapitalization Project, Navy Region Southeast.
- U.S. Department of the Navy (2013b). Environmental Assessment Wharf C-2 Recapitalization at Naval Station Mayport, Florida.
- U.S. Department of the Navy. (2015). Request for an Incidental Harassment Authorization Under the Marine Mammal Protection Act for the Wharf C-2 Recapitalization Project, Navy Region Southeast. April 2015.